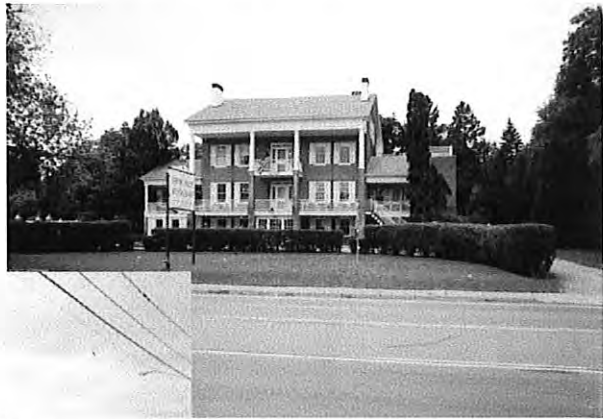
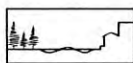


Town of Pittsford
Monroe County, New York
Monroe Avenue
Design Guidelines

(an appendix to the Town of Pittsford Zoning Code)



April 2, 2002



Behan Planning Associates
Planning Community Futures

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INTRODUCTION

The Monroe Avenue Commercial Corridor Design Guidelines help implement the Town of Pittsford Comprehensive Plan Update and Generic Environmental Impact Statement and are attached as an appendix to the Pittsford Zoning Code. The design guidelines were developed to be used by interested property owners, developers, and the public officials responsible for reviewing projects in the corridor. The design guidelines are written to be compatible with the town zoning code which shall be referred to as necessary and appropriate.

DESIRED STATE

These guidelines are directly aimed at maintaining and enhancing the unique history and emerging sense of place along Monroe Avenue. These policies recognize five unique character areas: from the gateway at the Pittsford-Brighton Town line in the Old Erie Canal area-to the Commercial Plaza area-to the Residential Transition (MATZ) area-to the village gateway at the Erie Canal Bridge. For each of these areas there are a set of specific, detailed, architectural and site design guidelines which govern the physical design and layout of the area, as well as, overall site and architectural design guidelines for the entire corridor.

It is envisioned that these design guidelines will aid the transformation of Monroe Avenue, as “a Main Street for Monroe County” from a typical commercial corridor to a truly great “Main Street”. Monroe Avenue in Pittsford will offer an interesting, cohesive, and attractive setting that complements the design themes of the other communities and neighborhoods located along the avenue. People living and working along the street will recognize and enjoy the distinctive character areas that comprise the avenue, while those traveling the avenue will experience a sense of aesthetic unity and attractiveness that makes Monroe Avenue a pleasant route to travel.



Digitally enhanced photo illustrating suggested improvements to the MATZ area. Photo courtesy of Mike Voelkl.

INTENT AND PURPOSE

The overall purpose of these guidelines is to address the way buildings relate to the public realm which exists along the avenue and to address the physical appearance of the corridor from area to area and as a cohesive unit. It is the intent of the guidelines to provide direction in creating an appropriate aesthetic experience for the look and feel of the landscape and buildings and, importantly, how each building can create a welcoming statement while providing clear definition of its entrance. It is the intent of these guidelines to allow flexibility in their application—where creativity and sensitivity will result. These guidelines will also:

- Support good development and community character-building ideas, including those from other communities;
- Create incentives for investment with flexibility and creativity in applying these guidelines;
- Incorporate adaptability to accommodate the alteration of existing structures and permit well designed new buildings that improve the pedestrian and visual environment.

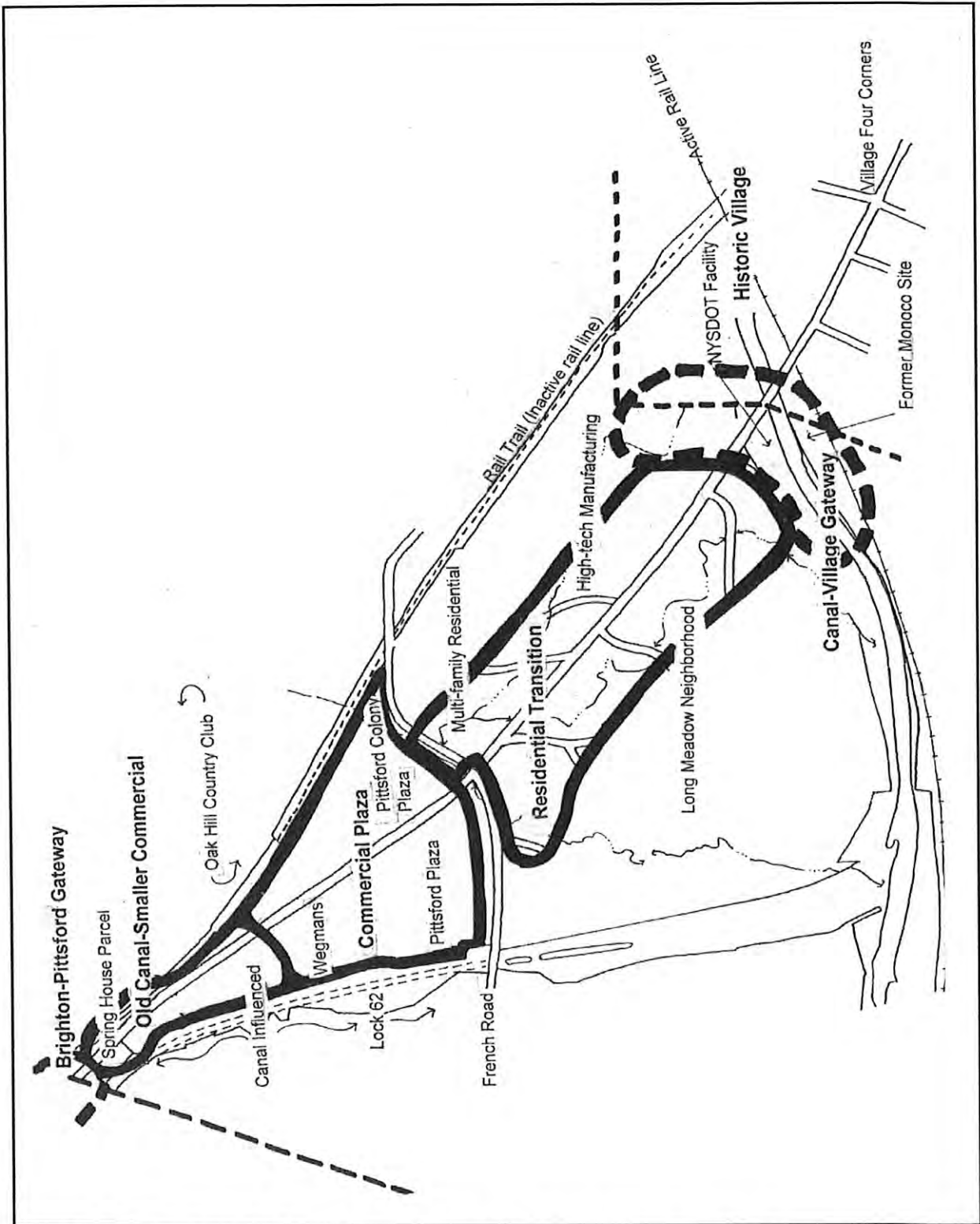
ORGANIZATION

The design guidelines are organized around a set of general principals which apply to the entire corridor. These general principals are then broken down into site and architectural design guidelines, first at a corridor-level, and then at an sub-area level. Like the principles, the corridor-level design guidelines are general in nature applying universally across the entire corridor. At the sub-area level, more detailed design guidelines have been crafted. In these sections, the aim of the design guidelines is to bring about the transformation of these areas to a desired character. The desired state of these areas and their specific design guidelines can be found at the rear of this document. The sub-areas include:

- The Gateway at the Pittsford-Brighton Town line;
- The Old Erie Canal Area - the area from the Gateway at the Pittsford-Brighton Town line east to the western entrance to Wendy's Old Fashioned Hamburgers restaurant;
- The Commercial Plaza Area - the area from the western entrance to Wendy's Old Fashioned Hamburgers restaurant east to 250 feet west of French Road;
- The residential Transitional Zone (MATZ) Area - the area from 250 feet west of French Road to the Village Gateway at the Erie Canal Bridge; and,
- The Village Gateway at the Erie Canal Bridge.

It should be noted that it is the intent of the design guidelines to supplement not supercede town zoning regulations, town zoning shall be adhered to for all proposed projects within the Monroe Avenue corridor. Sponsors proposing projects shall apply the design guidelines in the following order. First, adherence to town zoning, second to the corridor-level design guidelines and last by sub-area where

the project is located. Additional applicability requirements are outlined in the next section.



Graphic depicting each of the sub-areas along the Monroe Avenue corridor.

APPLICABILITY

It is expected that these guidelines will be used to assist in the design and review of new construction and alterations to existing sites and structures within each sub-area. Adherence to the guidelines is expected to the maximum extent practicable for all new construction. Practical difficulties or potential conflicts should be discussed with town staff with the intent that solutions reached will be brought to the appropriate review board for concurrence. With this said, it is the intent of the guidelines to provide substantive direction while providing reasonable flexibility, as determined by the appropriate review board, in their application. Generally it is expected that new construction and alterations to existing sites and structures be compatible with adjacent structures and the immediate neighborhood with respect to scale, mass and detail. However, where existing or adjacent structures are not in accordance with the design guidelines, new construction or alterations need not be compatible with said structures.

New Construction

The guidelines will be used to assist in the design and review of new construction projects, special permits, and site plan reviews in the corridor. Site plan approval is typically required for all new commercial buildings or expansions of commercial buildings within the corridor prior to construction.

Alterations to Existing Sites and Structures

The guidelines apply to projects involving modifications to existing sites and structures. Where a change of building occupancy occurs, the primary use is similar and no significant physical change is made, the guidelines shall not be triggered. Where guidelines are applicable, all alterations must comply to the maximum extent practicable. For the purposes of these design guidelines, the term alteration is defined as: any change (including significant paint or material color), rearrangement or addition to a building or site, other than routine repairs; a modification in construction; or in building equipment or the moving of a building or structure from one location to another. All requests to alter existing sites and structures must be submitted to the Code Enforcement Officer for review and possible referral to the appropriate board(s) (planning, architectural review, etc. as applicable). Permission must be granted prior to undertaking any alteration or construction.

Town Zoning

The guidelines apply to all new construction projects and alterations to existing sites and structures and will be used to assist in the design and review of new construction projects, special permits, and site plan reviews. However, where zoning regulations clearly establish the use, dimension, and/or other requirements such as performance standards for signage, zoning shall take precedence over the design guidelines. Where no such regulation exists within the zoning, the design guidelines shall be applicable. Whenever the design guidelines are applicable, compliance is expected to the maximum extent practicable.

PRINCIPLES

Successful places and memorable communities evolve from a set of guiding principles. These principles direct how an area will grow, evolve over time and adapt to changing situations. The principles which serve as the foundation for the Monroe Avenue Corridor Design Guidelines are derived from traditional principles of town planning and design and are intended to support the evolution of Monroe Avenue as a distinct and vital corridor. It should be noted however, the intent of the design guidelines is not to recreate another place but rather to apply traditional design principles, to make the Monroe Avenue corridor a vibrant and diverse place in its own right. The principles are as follows.

Relate buildings and sites to the “main street” frontage along Monroe Avenue.

- All new buildings and alterations shall be organized in such a way that the front of the structure addresses the public realm (e.g., front doors on front walks, engaging window displays and treatments [not including signs], etc...) which exists along the avenue. New buildings and alterations shall be located along the building line established for that area.
- Buildings sited along Monroe Avenue should have an entranceway that clearly and attractively presents itself to the traveling public.

- The primary building entry should face the street, with secondary entries facing the parking lot to provide access for patrons, employees and deliveries. The creation of additional side and back entrances to buildings will render side and rear parking lots more attractive to customers and the buildings more visually interesting to pedestrians. Front entries will invite patrons traveling along the avenue.



An example of a building which clearly addresses the public realm on the street, while providing secondary access to side and rear parking lots.

- Area setbacks throughout the Monroe Avenue corridor shall be sufficient to provide for a mix of public, semi-public, and private spaces and uses as appropriate for each setting.
- Public access such as, sidewalks and building entry areas, shall be incorporated into the site design of all development which front the avenue. Such provisions shall allow for pedestrian corridors and other design elements such as plantings, benches, trash receptacles, bike racks, signage, street trees and other design elements as appropriate and defined by the design guidelines.
- New construction shall present a human-scaled built environment.

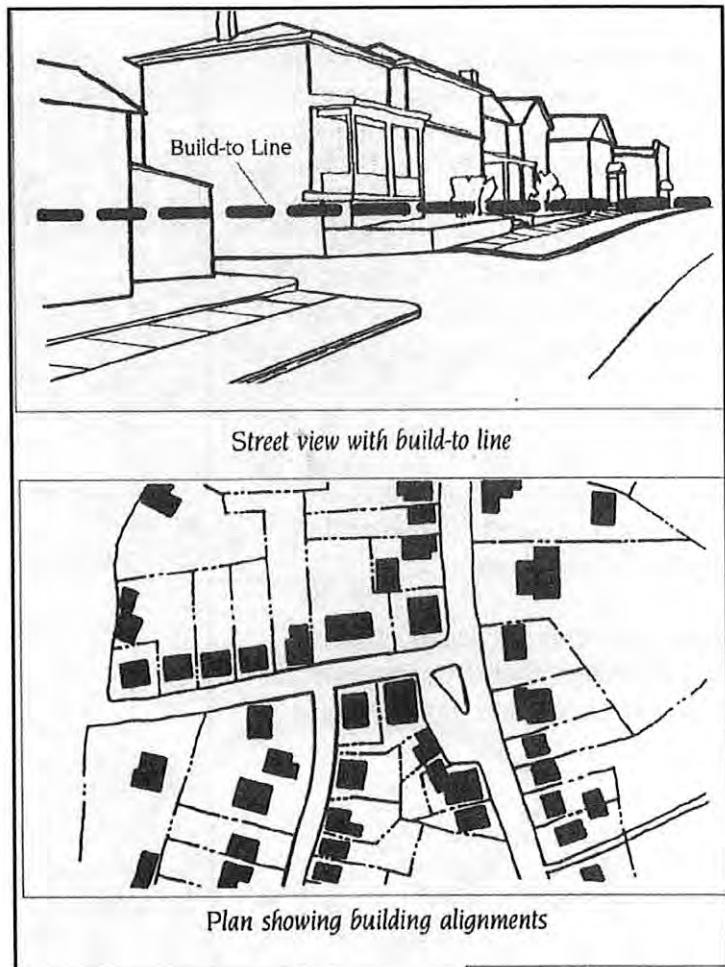


The building's relationship to the sidewalk, signage, planters, flags and banners, interesting window displays, and street trees all contribute to the human scale and lively atmosphere of this area.

Apply traditional design patterns which respect area and building scale, patterns, and details.

Encourage the application of traditional design patterns in development projects within the Monroe Avenue corridor. Such patterns include the following elements.

- Attractive buildings that are designed in accordance with and sympathetic to local vernacular architectural styles and details.
- A “continuity of interest” along the street of landscaping, unique architecture, and attractive and engaging facades, entrances and display windows in retail areas.
- A consistent “build-to” line for commercial buildings maintaining a uniform streetwall.
- Buildings designed to maintain and respect the size, scale, pattern and detail of historic structures and the vernacular architecture in the area by using materials, such as brick, wood, and wood shingle siding.
- Provision of attractive public spaces (plazas, courtyards, squares, open space) and pedestrian accessways.
- Parking lots, service entries, storage, maintenance, loading, and refuse collection areas relegated to the rear or sides of buildings screened from public view.



Graphic illustrating the use of a “build-to” line in creating a uniform streetwall.

- In addition to site layout and building configuration, buffer plantings, appropriate fencing, or a combination of the two shall be employed to visually screen parking lots, service areas, utility facilities, dumpsters and other unsightly elements, as well as to mitigate noise and offensive odor impacts on adjacent properties or nearby residential areas.

Create clear vehicular movement and pedestrian patterns.

The Monroe Avenue corridor shall accommodate pedestrians, bicycles, and motor vehicles.

- The corridor and all internal drives shall be lined with trees, sidewalks, low-level lighting, and landscaped areas with trees, shrubs, flowers and ground cover.
- Internal drives and walkways which connect to the avenue shall form an interconnected network to distribute automobile traffic. These drives shall link and unify the uses in a project and adjacent commercial properties, while providing an attractive and safe environment for pedestrians.
- Sidewalks shall be an integral component of the transportation infrastructure within the corridor, as shall bike and/or multi-purpose trails.
- The trail system shall loop the entire corridor and be used to strengthen the connection between neighborhoods and the Monroe Avenue corridor.



A bus pull-off, benches and a shelter would make this bus stop more user friendly and safe.

- Connections between uses are encouraged and shall be considered when siting adjacent buildings in the corridor.
- Access to community based transportation (e.g bus service) shall be provided at visible, attractive and safe locations within the corridor.
- Vehicular and pedestrian movement shall be separated through the installation of a pedestrian friendly streetscape (i.e., A streetscape that includes: a conscious and visible attention to pedestrians; provides ease of movement and safety; and, which contributes to the aesthetics of the corridor). In areas where vehicular and pedestrian conflicts occur, or may occur, pedestrian amenities shall be upgraded to an equal or greater priority than vehicle movements.
- Vehicular and pedestrian safety may be promoted through the use of traffic calming devices. Conflicts and confusion should be discouraged through adequate internal travel lanes edged with curbs, plantings and clear signage.

ARCHITECTURAL ISSUES***Building Height***

- New construction shall not compete with, nor destroy the street line and skyline but shall enhance its overall character.
- The height of new construction shall complement the size of adjacent structures which are compatible with the design guidelines.

Roof Lines

- Peaked roofs are generally preferred to flat roofs.
- Creative use of gables, dormers, and other roof line elements can be used to highlight entrances and bring a sense of architectural distinction to the building.
- Antennas, satellite dishes, air handling units, exhausts and other mechanical equipment placed on the roof shall not be visible from the street, parking areas or the internal roadways and sidewalks.
- The roof of a structure shall be designed so as to divert the fall of rain and snow away from the pedestrian areas such as walkways and front doors.

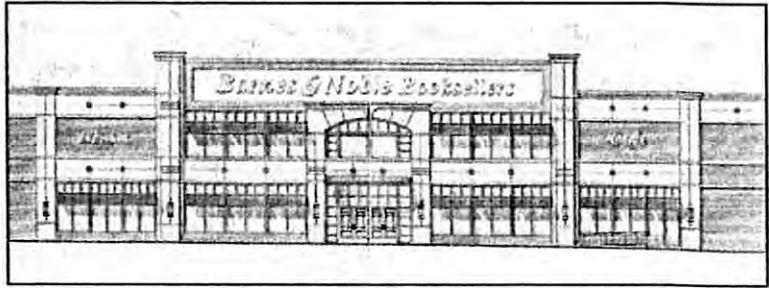
Building Proportion and Scale

- Materials such as brick, stone and wood clapboard which are smaller in scale and which compliment the character of the avenue should be used on the street front and in other pedestrian areas instead of metal or glass panels. When regarding the buildings around a particular project, consideration should be given to a diversity of material types and subtle colors to avoid overwhelming the avenue with one kind of siding or roofing material.

Fenestration (i.e. doors and windows)

- The amount of fenestration in a facade shall be consistent with the amount of fenestration in adjacent buildings and shall remain sympathetic to the buildings in the vicinity and respond to the local architectural styles.
- At street level, the ground floor facade and in particular the entry, shall have the highest amount of fenestration and depth, shall be open and inviting, and shall have large display windows to glance into.

- The facade on the upper floors shall be complementary to the ground floor facade with the upper floors having slightly less fenestration and a decreasing percentage of window openings.



This graphic illustrates complementary ground and upper floor facade treatments.

- Openings in the facade shall be framed by a lintel or arch above doors and windows and a sill at the bottom for windows. Lintels in masonry buildings shall appear strong enough to support the weight they are carrying.
- Windows shall be set back in the wall to highlight them rather than mounted flush.
- Smaller windows shall be framed within a recess to make them appear larger, or grouped together in two's so that they may appear to act as a pair.

Building Materials and Colors

- Building color shall complement colors used on adjacent structures and be of a natural, muted shade.
- The use of constants between the main color theme and an accent color are recommended (e.g., dark vs light), however, the number of colors used should be kept to a minimum.
- Subtle accents in the plane of the facade, created through the use of secondary materials such as rough textured materials or decoration (such as brickwork patterns), are encouraged but should not be used as primary theme.
- When using multiple materials in a facade, a clear and definable boundary (such as a small ledge) should be used to terminate one material before making the transition to the next.
- Finish materials and colors on alterations shall match or complement those original to the existing buildings (note-many structures and additions currently display finish materials like siding and paint colors which are not those originally used). Where the original material is out of character, inferior to or incompatible with available materials,

the more appropriate material shall be used.

- Original details should be preserved and restored whenever possible and alterations shall not be significantly more articulated or more elaborately detailed than the original structure.
- As an alternative to matching existing materials, finish material and colors should be selected from those commonly used at the construction date of the original structure.

SITE ISSUES

Transportation and Mobility

Monroe Avenue plays an important role in the transportation network of the Town of Pittsford and the larger region. The avenue is both a major regional transportation arterial as well as a collector for local businesses and residents. The town recognizes the need to maintain existing levels of service along the avenue while providing for the safe and efficient movement of both pedestrians and vehicles into and out of the area.

Clear patterns of vehicular and pedestrian movement are essential to creating a safe and pedestrian friendly environment. To ensure the safe, efficient movement of pedestrians, bicyclists and vehicles, modes of transportation shall be separated through the use of a well-defined streetscape (including sidewalks for pedestrians and dedicated bicycle lanes for cyclists), and the use of traffic calming mechanisms and road improvements.

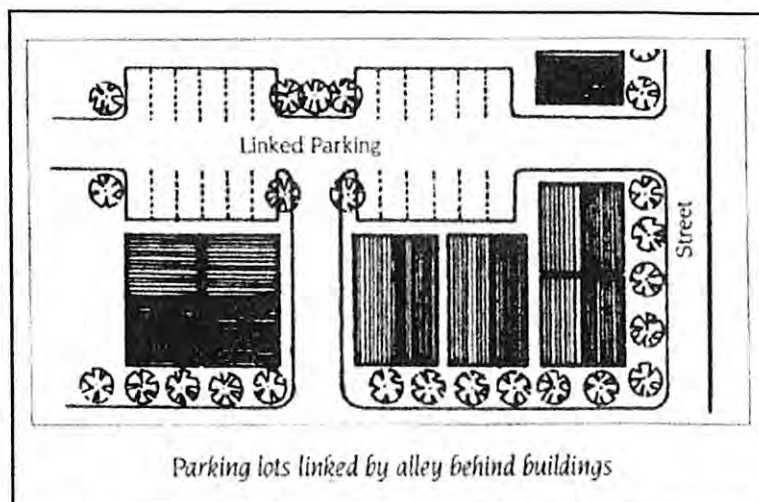
Pedestrian and streetscape elements and traffic calming mechanisms are addressed under the section Streetscape and Traffic Calming. Road improvements to the corridor should include:

- Clear, uniform and safe intersection alignments; and,
- Incorporation of traffic calming mechanisms.

Linkage and Curb Cuts

The clear and orderly organization and movement of pedestrians and vehicles can be further enhanced by linking destinations via a pedestrian network, linked parking areas, and by the use of curbs to reinforce the separation of vehicular and pedestrian areas. Therefore:

- Excessive curb cuts should be eliminated or reduced;
- Curbs should be installed to define the edge between parking areas and roadways, and;



An example of shared access and parking which can be used to maximize development potential.

- Pedestrian and vehicular connections should be made between stores and parking areas.

Streetscape and Traffic Calming

Safe and efficient movement of vehicles and pedestrians can be accomplished through a well defined streetscape. Such a streetscape creates an attractive environment with clearly defined pedestrian, vehicular, and shared or overlapping zones. Traffic calming mechanisms may be coordinated with streetscape and road improvements. Together these improvements introduce visual elements along the road edge which alert drivers to increased activity and the presence of pedestrian traffic.

It is strongly recommended that sponsors and agencies proposing construction projects or alteration to trafficways in the Monroe Avenue corridor, consider all modes of traffic and parking as an integrated whole. For example, any project cross-sections should include all elements of the public realm, including the transportation and parking system from the edge of buildings—not merely a cross-section of road, parking lot, sidewalk or trail.

In areas of shared movement, traffic calming mechanisms can be used to manage the overlap between pedestrian and vehicular zones. These shared areas occur most frequently along road shoulders, at intersections and crossings, and in parking lots. Traffic calming elements include:

- The realignment of dangerous intersections to provide clear sightlines;
- Incorporation of curves into street design;
- The narrowing of travel lanes;



Elements of a pedestrian oriented streetscape include curb bump-outs, crosswalks, planted medians and sidewalks. As an alternative to tradition striped crosswalks, materials such as concrete, brick or pavers, may be used to further emphasize pedestrian areas such as crosswalks within a roadway.

- Changes in pavement materials (e.g., concrete crosswalks), texture, color, pattern, and/or striping (e.g., crossing bars) at crosswalk locations;
- Construction of sidewalks, curbs and curb bump outs;
- Incorporation of pedestrian cross-walks;
- Incorporation of speed bumps, humps and rumble strips;
- Use of street trees;
- Use of appropriate signage, lighting, pedestrian crossing signals, traffic lights, and;
- Clear boundary or transition zones which define and mark road edges.

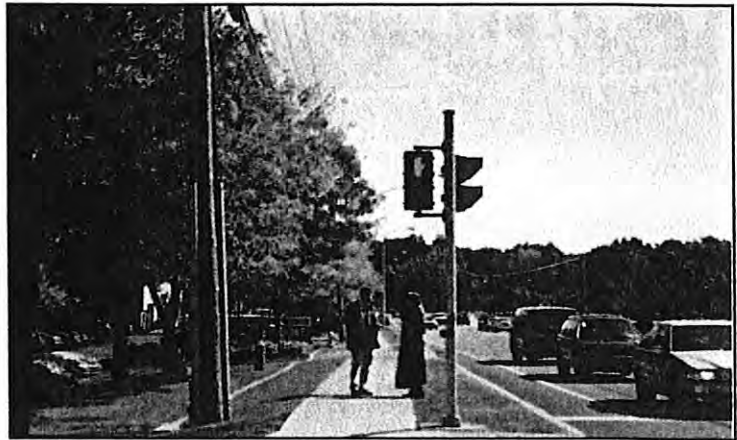


As an alternative to traditionally striped crosswalks this example shows large painted bands or “crossing bars”.

Parking and Pedestrian and Vehicular Circulation

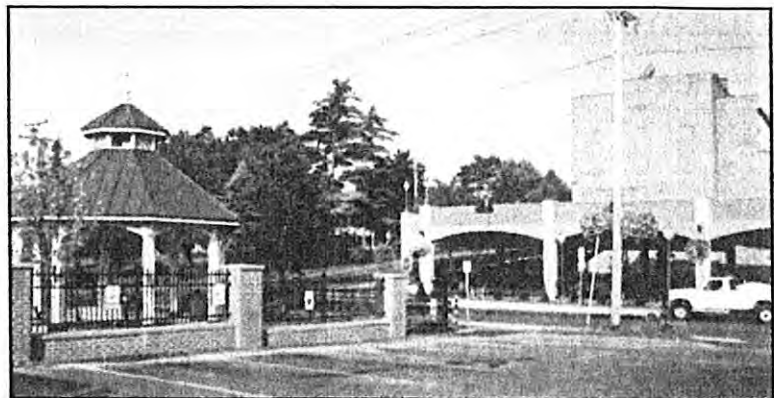
- The design of parking lots shall provide for the safe and efficient movement of both pedestrians and vehicles.
- Parking shall be subservient to the buildings and pedestrian system.
- Parking requirements, as addressed in the town zoning code and specific to the sub-area (for example, parking space restrictions in the MATZ sub-area), shall be adhered to for all new development and construction projects.
- Parking requirements and a parking plan shall be submitted by the applicant with the project sponsor’s proposal. For the review of site plans where no specific standards exist, the planning board will determine the appropriate parking count. The goal is to provide adequate but not excessive parking spaces and paving. For questions of parking determinations, the code enforcement officer should be consulted. Such determination will be issued prior to final site plan approval.

- Shared parking to reduce excessive pavement and to increase the building-to-site ratio is encouraged and will be considered in site plan review. The town may require proof of shared parking agreements. The applicant is well advised to work with professional staff from the town to determine parking adequacy.



Vehicular and pedestrian separation.

- To help increase vitality and building space, structured parking (e.g., a parking deck) may be used. These structures must be sited so as to be unobtrusive when viewed from Monroe Avenue and the surrounding pedestrian environment.



The structure on the right is a two-story parking deck.

- Planting and maintenance of a grassed areas of adequate width, to separate the pedestrian sidewalk from the vehicle travel lane and accommodate plantings, lighting, signage and snow storage.
- Sidewalk corridors shall relate to building and store front entrances, link streets with parking lots and provide for safe efficient pedestrian movement.

- Sidewalks shall be built to town construction standards and incorporate Americans with Disabilities Act (ADA) standards for handicapped individuals. Concrete or pavers are the preferred materials for sidewalks and crosswalks and where major sidewalks cross streets, driveways, and parking areas.



Landscaping and Street trees

The character of Monroe Avenue shall be enhanced through the use of landscaping as part of a streetscape and road improvement program. It has been demonstrated that pedestrian-oriented environments which include the use of landscaping as part of a streetscape improvement program generate more activity and interest along the road frontage. Therefore, all landscaping and landscape treatments shall enhance the pedestrian experience in the corridor.

Street trees are recommended in the corridor area for several reasons. First, they shade and enclose the street and pedestrian areas. Second, because of their vertical nature, they act as walls further defining the transition zone between vehicular and pedestrian corridors of movement. Third, by virtue of their location along the street and their vertical nature they reduce the vast physical space which exists between building setbacks on either side of the street. Last, by reducing the physical space, smaller more intimate areas of space which relate better to human dimensions are created.

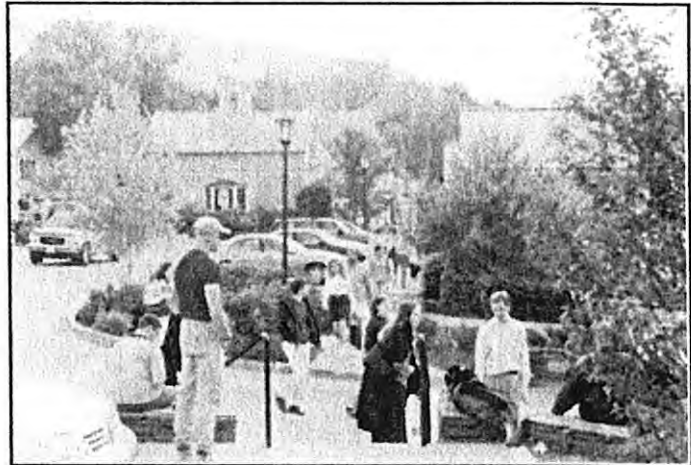
The corridor’s landscaping shall include: street trees, planters, buffer plantings, parking island plantings, as well as, individual establishment’s landscapes which are designed in harmony with the area’s overall theme. Trees, shrubs, flowers, and ground covers which are visually appealing throughout the seasons, responsive to the three-tier spatial qualities of plantings, including ground, vertical and horizontal/overhead plane, and hardy for the area’s climate shall be used as appropriate. In areas where the long term mature plantings are impractical, container plantings are encouraged, provided they are tasteful and strategically located. Overall landscaping and landscape treatments, in the corridor, shall:

- Maintain views of vehicular and pedestrian movement corridors;
- Be visually appealing and hardy;
- Provide visual relief, shade, and;
- Buffer areas between adjoining land uses.



Additional guidelines include:

- A reserve area, of adequate width, shall be designated for street tree planting in all developments.
- The removal of healthy street trees and mature trees shall be discouraged and efforts shall be made to preserve existing street trees.
- Street trees which are integral with, or adjacent to, pedestrian walkways shall be limbed to a standard height which allows for a clear walking zone.
- In areas of high pedestrian traffic protective measures such as curbing to define tree planters, tree grates to allow infiltration of water and protection of roots, trunk protectors and guide-wire shall be used.
- In parking lots, large areas of asphalt shall be divided into smaller units through the use of landscaping or other techniques.

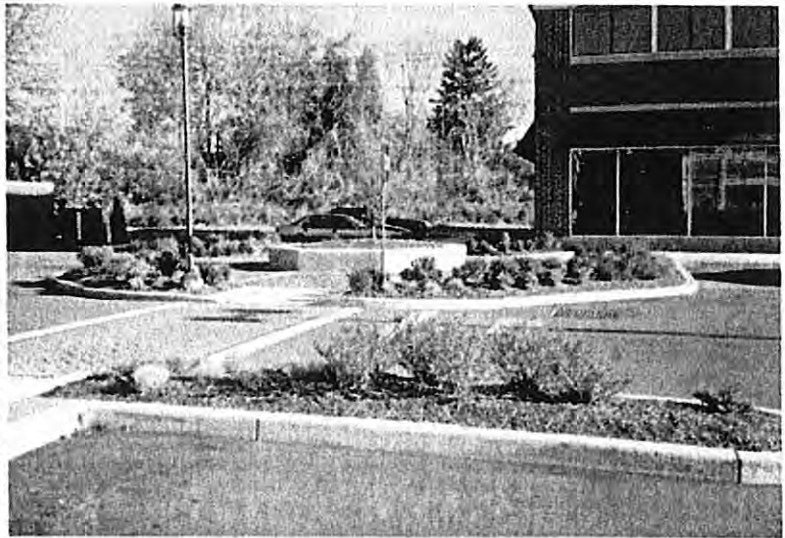


An example of the use of vegetation to screen undesirable views, break-up large expanses of pavement and contribute to the character of development.

- Planting islands shall be large enough to support mature trees and snow storage (typical width 6 - 8 feet).
- Trees and other plantings in parking lot islands shall be tolerant of salt, cold temperatures, and be physically strong enough to support a load of snow without additional protection.
- Plantings shall provide visual relief, shade and buffer adjoining land uses in parking lot areas.
- The removal of healthy mature trees and vegetation within project sites shall be discouraged and efforts shall be made to preserve all such trees and vegetation. Existing vegetation, particularly large trees, shall be inspected by a qualified arborist (tree specialist) to determine general health conditions.
- Plantings and street trees shall be maintained. Overgrown plantings shall be pruned, and, where appropriate, removed and replaced with new plantings.

Open Space and Amenities

In the entire Monroe Avenue corridor, both new development projects and existing commercial enterprises undergoing alteration, other than routine maintenance, shall create active and attractive pedestrian-oriented and human-scaled open spaces to improve the aesthetic nature of the area and preserve existing open spaces. Existing open spaces in the corridor include: the lands associated with the Erie Canal, the Old Erie Canal, and the Auburn Rail Trails. Open spaces and their associated planting areas therefore are required,



as an integral component of the design scheme, rather than a remnant of the development process in both new development projects and alterations to existing commercial areas. Types of appropriate open spaces include but are not limited to plazas and courtyards. Therefore, new development and alterations shall:

- Provide physical, where feasible, and visual access to significant viewsheds and resources.

- Incorporate additional open spaces and buffers, where feasible, to provide transitional areas between public and private spaces.
- Maintain the condition of existing trails, trail connections and access points.

Lighting

- Lighting fixtures shall be of adequate scale, wattage and mounting height to provide appropriate illumination in corridor sub-areas. The applicant is encouraged to work with professional staff from the town early in project review to ensure consistent lamp styles where appropriate and the provision of adequate, not excessive, lighting levels. Town staff may rely on published electrical lighting standards for these reviews.
- Continuous illumination shall be provided along Monroe Avenue, interior street networks, pedestrian walks and, if appropriate, trail system segments.
- Parking and circulation lighting fixtures shall include a cutoff type luminaire to prevent spillage of direct light above the fixture or onto adjacent property if such spillage causes a nuisance on neighboring properties. In no case shall light spillage cause glare to be directed into driving lanes on Monroe Avenue or internal roadways.
- Where appropriate, the intensity levels of individual fixtures may be reduced by utilizing a greater number of lower intensity lamps. In any event, the fixture style shall be subject to the approval of the Architectural Review Board (ARB) and the Commissioner of Public Works. The illumination type, height, intensity and number of lights shall be subject to the approval of the Commissioner of Public Works or his



Lighted bollards can be used to create a separation between pedestrian and vehicular movement as well as to provide lighting for pedestrian and trail systems.

designee. Detailed illumination plans may be required for review of applications. These studies, when required should not be undertaken by a sponsor prior to the resolution of the larger of project layout, parking and circulation. Particular attention should be paid to minimum/maximum ratios and average lumin values for the specific use proposed, according to recognized authorities (e.g., the Illuminating Engineering Society of North America). Notwithstanding the preceding, the Commissioner of Public Works may request specific lighting types and levels if deemed important to the maintenance of sub-area character. The lighting plan must be approved prior to or concurrent with the issuance of final site plan approval.

Signs and Awnings

- Signs and awnings are subject to specific restrictions and requirements identified in Article XVIII of the Town of Pittsford Zoning Code. Signs shall fit the design palette of the sub-area where they are proposed to be located.
- Signs shall be easy to read, should clearly express the nature of the activity occurring on the premise, and be complementary to its surroundings and not be garish or distracting.
- Signs shall not be constructed in excess of the scale established in the Town's zoning code. For example, the allowable type and size of signs in the MATZ sub-area are substantially smaller than those allowed in the retail commercial sub-areas.
- Buildings or their architectural treatment shall not be so garish in line, color, or effect, so as to constitute a sign in themselves.
- Painting, striping, lighting, or other graphic or attention getting device, is considered a sign and is regulated by town zoning and these guidelines. Therefore, no structure shall be so striped or painted, so as to make the structure or building a sign.
- Where signs are designated to highlight walking or multi-use trails, a centrally located directory shall be placed in a suitable area within the corridor. The directory shall contain a map directing pedestrians to points of interest in the larger community and other community information.

ANCILLARY COMPONENTS

Ancillary components are important additions to the streetscape. These components play a significant role in enhancing the visual character and sense of place of the Monroe Avenue corridor. Examples of ancillary components include, but are not limited to, bicycle storage racks, water fountains, benches, tables, trash receptacles, and tree grates. These components should be coordinated with other

streetscape elements, require low maintenance, and be fairly durable.

DETAILED DESIGN GUIDELINES FOR EACH SUB-AREA

Monroe Avenue in the Town of Pittsford can be thought of as comprising several relatively distinct sub-areas. These sub-areas provide the basis for creating a set of design themes for guiding beneficial change which will occur along the avenue. The sub-areas are:

- The Gateway at the Pittsford-Brighton Town line;
- The Old Erie Canal Area;
- The Commercial Plaza Area;
- The Residential Transition (MATZ) Area; and,
- The Village Gateway at the Erie Canal Bridge.

OLD ERIE CANAL SUB-AREA

Beginning at the Gateway at the Pittsford-Brighton Town line east to the western entrance to Wendy's Old Fashioned Hamburgers restaurant, and centered about the Spring House and Old Erie Canal corridor, this area offers a canal-era theme and architectural character. The smaller sites are or can be over time integrated with architectural styles that complement the historic style of the Spring House, a property of great significance to the town, region, and nation. The 1822 Spring House is listed on the National Register of Historic Places. It was a popular stop on the Old Erie Canal because of the natural mineral springs located across Monroe Avenue from the Spring House. Today the original springs reside on the Oak Hill golf course property.

The design challenge is to balance the preservation of historically significant sites and structures with new construction and alterations and for new construction and alterations to be sympathetically designed so as to enhance the historic character of the area. Restaurant, retail, offices and cafes shall be the preferred uses for new business locating in the sub-area.



Bushnell's Basin is an example of a historic, pedestrian-friendly area which incorporates a variety of uses from restaurants, to retail.

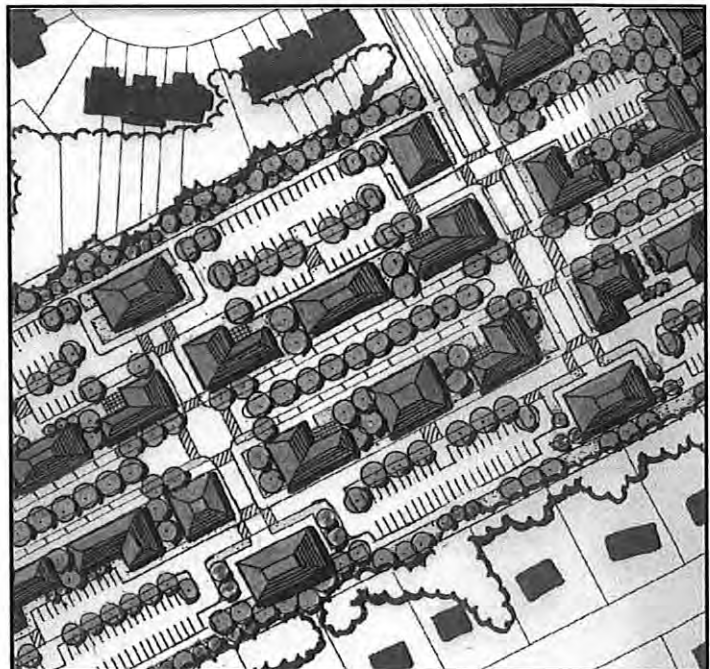
ARCHITECTURAL ISSUES

Historic Buildings

- Where the structure being altered is a building of historic significance or listed on a local, and/or state and national register of historic places, adaptive reuse and appropriate additions, as outlined in the U.S. Secretary of the Interior’s standards for historic preservation, are strongly encouraged as an alternative to demolition.
- Where said structure is located within a historic district, and/or listed on a local, state and national register of historic places then additional regulations will apply. It is the responsibility of the application to demonstrate compliance with these regulations at site plan review.
- A certificate of appropriateness from the Historic Preservation Commission is also required prior to any change in the existing structure as referenced in the Town of Pittsford Zoning Law, Article XXX, Landmarks and Historic Districts.



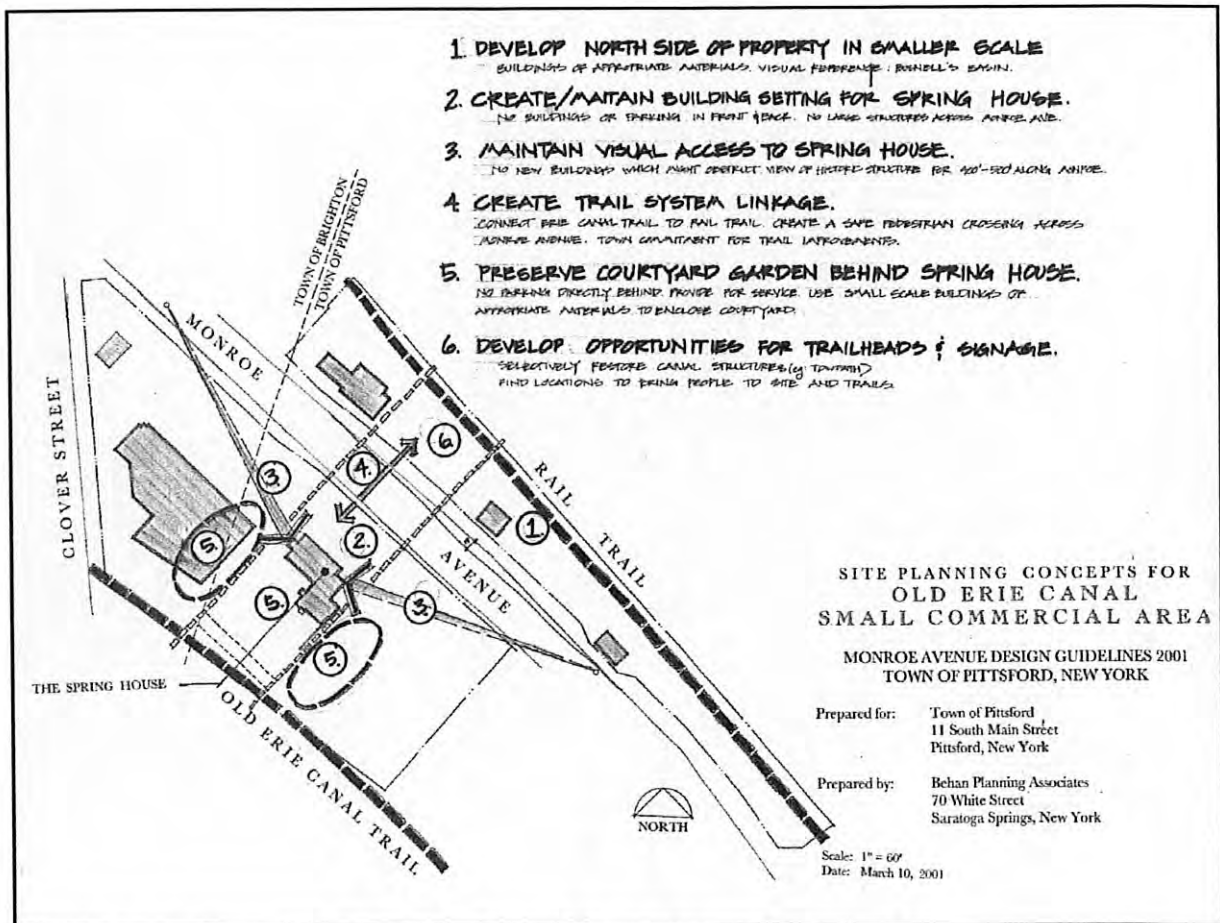
The Richardson Canal House complex provides a nice model for a design palette for the Old Erie Canal Sub-Area on Monroe Avenue



This graphic conceptually illustrates how buildings can be placed as a terminus to significant lines of sight.

Building Siting

- Sight-lines shall end on important visual elements, where feasible, such as



Graphic depicting approximate extent of the Spring House viewshed (#3).

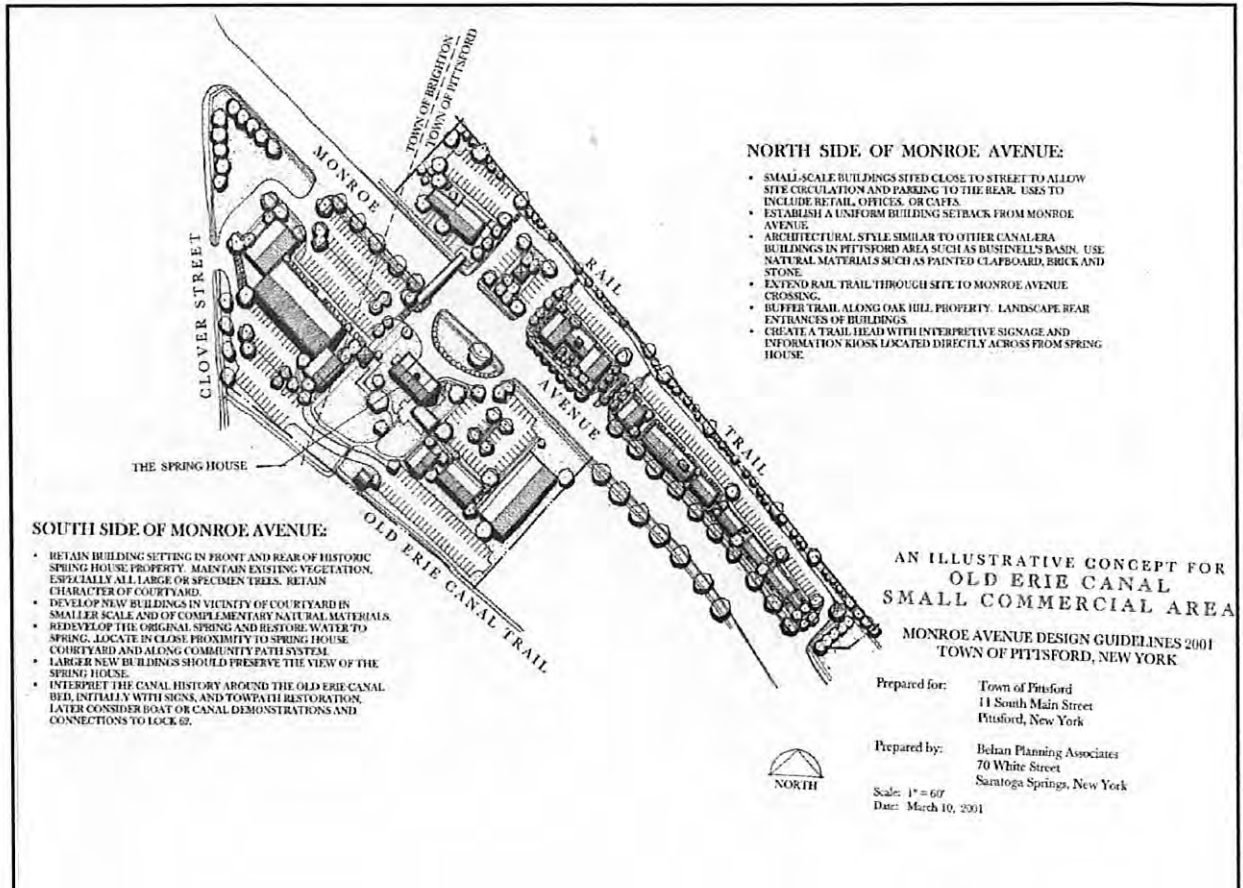
significant buildings, or public spaces, never end on a blank wall.

- The Spring House viewshed, extending approximately 400 ft in either direction from the Spring House along the southern side of Monroe Avenue, shall be preserved and unencumbered by new building construction. Preservation of the viewshed shall supercede required setbacks along



the south-side of Monroe Avenue within the sub-area (see graphic).

- Along the north-side of Monroe Avenue only, buildings shall be located as close to the highway right-of-way as feasible.
- Open space, courtyards, and landscaped pathways shall be encouraged to provide transitional areas between public and private spaces, such as the Old Erie Canal or Auburn Rail Trails.
- New construction within the sub-area shall accommodate and not encumber the Old Erie Canal or Auburn Rail Trails nor their linkages.



Graphic depicting preferred development concept for the Old Erie Canal sub-area. Graphic is illustrative in nature and is not representative of the final site development plans.

General Architectural Standards

- New buildings shall emulate the architectural style and character of earlier canal-era buildings, such as those found at Bushnell's Basin.
- Diversity that is in tune with the scale, mass, proportion, decorative design elements (such as roof shape, gable orientation, window size, shape, and spacing, and exterior materials), and street relationships of traditional buildings, are encouraged.



Building Height

- 1 ½ to 2 ½-story buildings are encouraged. The height of new construction shall complement the height of its neighboring buildings and shall not compete with it. In the vicinity of the Spring House, the height of the Spring House shall dominate all other structures.
- In areas where more height is desired, additional height can be implied by using strong vertical proportioning elements on the facade.



An example of a suitable 1 ½-story building.

Roof Lines

- Extensive use of very steep, or flat or very low pitched roofs shall be avoided.
- Where flat roofs are used they shall be capped by a structural expression of the facade, not a false roof front.

Fenestration (i.e. windows and doors)

- Window types above the base level shall be double hung, awning or transom windows on street elevations. Picture and sliding windows are not permitted.
- Pairs of window shutters may be used, but should be consistently used and should appear to be actually over the entire window opening when closed.

Building Proportion and Scale

- Clusters of smaller buildings with internal open spaces are desired, rather than large single buildings separated by vast expanses of parking lots.
- Where feasible, buildings shall be scaled down into groupings of smaller structures to avoid bulky masses and to create smaller, human-scaled environments. This can be achieved by designing breaks in the building mass and roof lines, and by including

appropriate architectural detail.

- New buildings, constructed in the vicinity of the Spring House Courtyard, shall be of a smaller scale and complementary in mass, proportion, fenestration, use of natural material etc. to the Spring House.
- The relative portion and size of design elements, such as windows and bays, in a structure are most successful when kept consistent throughout the design, and in keeping with the proportions, general scale and mass of adjacent structures.
- Larger scale materials and features, such as panels and blank walls should be reserved for the rear of building and out of sight from the street and pedestrian areas.

Facade Composition/Rhythm

- Buildings which face the street shall relate to the street through an interesting facade, entranceway, window treatment or scheme and never a blank wall.
- New facades should attempt to coordinate/complement the relative heights of elements (e.g., datum lines), rhythms or bay systems of adjacent buildings as they are expressed on the facade with adjacent buildings.
- The facade shall have a strong but simple pattern and should maximize window and door openings along the avenue, but need not be rigidly structured for side street elevations.
- Facade compositions should be made up of base, middle and top levels that are readily discernible from each other.
- The top level of a facade should be noticeable enough to provide a visual cap to the building overall.
- Vertical elements on the facade, such as columns, posts and pilasters, can be used to heighten an otherwise short building. However, these items should be proportioned so as not to appear too thin or spindly to carry the weight above them.
- The rhythm of the facade should be of a clear, simple and easily recognizable pattern. Within this pattern, however, more subtle and interesting patterns may be utilized to further enrich the design.
- Breaks and fluctuations in the overall rhythm of a building should be used to draw attention to important areas such as the entry, or to simply relieve the monotony of a

very long facade.

- Front porches may be used to create a strong rhythm and provide shelter for pedestrians.

Building Materials and Colors

- Within the sub-area, painted or stained clapboard and cedar shakes are the preferred material for new construction, however, brick may be used but not excessively.

SITE ISSUES

Linkage and Curb Cuts

- Look to consolidate curb cuts and realign entrances to match at both sides of Monroe Avenue.
- Encourage establishments to share parking areas, common entrances and exits, and to provide sidewalk connections between them.

Streetscape and Traffic Calming

- Buildings along the northern side of the avenue (Oak Hill side) shall be set close to the road.

Parking and Circulation

- Smaller shared parking areas are preferred and should be encouraged to the maximum extent possible.

Open Space and Amenities

- Views of the Auburn Rail Trail, and the rear of business, from the Oak Hill Golf Course property shall be buffered through the use of vegetation and fencing as appropriate.

- A trail head, complete with interpretive signage and information kiosk, and public access right-of-way or trail connection linking the sidewalk system on Monroe Avenue to the Auburn Rail Trail should be constructed across from the Spring House site.



The dumpsters in the foreground should be screened from view of the golf course through vegetation and fencing.

Lighting

- Lighting fixtures for parking lots shall be between 12-20 feet in height, depending on the setting.
- The lighting plan must be sympathetic to the historic flavor that guides the design of this area.
- Pedestrian lighting shall be on fixtures not exceeding 15-feet in height. Generally, light sources shall be relatively low to the ground in order to remain in scale with human dimensions and provide illumination beneath the canopy from trees. These can be freestanding fixtures located along the sidewalks. Luminaires without cutoffs are acceptable for pedestrian-level lights however, as a general rule all outdoor lightning shall be screened by shields or hoods to prevent glare onto adjacent premises.



View of the Spring House Courtyard.

SPRING HOUSE SITE

- The historic landscaped settings in the front yard and rear patio of the

Spring House site shall be preserved.

- The character of the Spring House Courtyard as expressed through its use of vegetation and plantings, natural materials, sense of enclosure, etc... shall be preserved.
- Incandescent pedestrian lights and low level parking lot lights, with a maximum height of twenty feet, shall be used on the Spring House Site.



View of the Historic Spring House from Monroe Avenue.

- A public access right-of-way or trail connection linking the sidewalk system on Monroe Avenue to the Old Erie Canal Trail should be constructed on the Spring House site. Such a trail will create a public benefit and increase economic activity to the area. Ideally, the trail should be extended through the Spring House site, along the towpath to Clover Street.
- Construction of a new spring and pavilion shall be encouraged on the Spring House site. The pavilion shall be located either in close proximity to the Spring House, or along an adjacent public access right-of-way or trial connection.

COMMERCIAL PLAZA SUB-AREA

Larger scale retail and commercial developments including landscaped parking areas and internal vehicular and pedestrian circulation systems, attractive storefronts and commercial buildings and landscaped areas that create an interesting and pleasant architectural edge along the avenue are permitted in this area. Pedestrian-level lighting should be incorporated within individual commercial developments and along sidewalks. The design challenge for commercial development within this area is to help maintain and improve the Monroe Avenue corridor for existing businesses and for new enterprises.

ARCHITECTURAL ISSUES

Building Siting

- Ideally the predominant element of the streetscape will be attractive buildings and engaging storefronts lining Monroe Avenue with limited breaks in this streetwall for parking and views to other buildings and public spaces.
- Site lines shall end on important visual elements, where feasible, such as significant buildings, or public spaces, never on a blank wall.
- Strategic openings in building lines shall be provided to allow visual access to important vistas and physical access to important public and semi-public spaces.
- Open space, courtyards, and landscaped pathways shall be encouraged to provide transitional areas between public and private spaces such as the Old Erie Canal or Auburn Rail Trails.
- Buildings shall be oriented to minimize mass as seen from Monroe Avenue.
- The front yard setback shall be used for plantings and/or an entry feature to make the store fronts more engaging and responsive



Open spaces and small plazas add to the character of the area and help connect buildings for pedestrians.

to the public realm of the street and to screen parking areas.

General Architectural Standards

- New construction shall feature elements such as open or enclosed porches, colonnades, patios or plazas and landscape amenities which are at a human scale.
- Old and new structures should appear as a comprehensive sequence in size and shape. This can be achieved through the use of architectural detailing to create variety and interest on new buildings.
- The design of new construction shall creatively reflect traditional elements of the local vernacular architecture, be compatible with (materials and pedestrian elements, building mass, scale, roof slopes, proportion and size of openings, architectural styles and details, and overall development density etc.), and positively contribute to the overall organization and architectural theme of the corridor and not detract from it.
- Buildings, building alterations and plantings shall form a definitive edge to the street instead of a dominance of pavement and parking lots.

Building Height

- Within the sub-area 2-story buildings are preferred. Where feasible, several smaller 2-story buildings shall be placed along, and relate to, the corridor's internal streets.
- 3-story buildings, where feasible, are also permitted. These buildings may be located at corner lots, at the intersection of Monroe Avenue and any internal street, and as a terminus to any intersection, as feasible.

Roof Lines

- Extensive use of very steep, or flat or very low pitched roofs shall be avoided.
- Where flat roofs are used they shall be capped by a structural expression of the facade, not a false roof front.
- Sloping roofs shall be broken up by the use of dormers and gables to give the facade more visual prominence.
- Longer buildings shall provide fluctuations in the roofline which are designed to break up the facade, and make entryways more prominent.

Building Proportion and Scale

- Clusters of smaller buildings with internal open spaces are desired, rather than large single buildings separated by vast expanses of parking lots.

- The overall facade composition can give a small scale impression and be sympathetic to the scale of neighboring buildings by: breaking the building down into smaller distinct portions; using subtle articulation and divisions between



These commercial buildings are good examples of how decorative designs, commercial buildings can appear smaller in scale by breaking and adornment in the the building up into smaller, distinct, more human-scaled facade; using proportions with a front door and display windows on first floor similarly sized of the building.

building materials and wall openings; and by coordinating with the datum lines of adjacent buildings.

- The proportion of new construction should be compatible with the proportion of its neighboring buildings or to a scale compatible to the smaller of the structures.



- The relative portion and size of design elements, such as windows and bays, in a structure are most successful when kept consistent throughout the design,

An example of a larger structure on Monroe Avenue which has been broken down visually into smaller units by the use of strong vertical elements and by stepping back the facade at set intervals.

and in keeping with the proportions, general scale and mass of adjacent structure.

- Larger scale materials and features, such as panels and picture windows grouped tightly together, and blank walls should be reserved for the rear of building and out of sight from the street and pedestrian areas.

Facade Composition/Rhythm

- Buildings which face the street shall relate to the street through an interesting facade, entranceway, window treatment or scheme and never a blank wall.
- New facades should attempt to coordinate/complement the relative heights of elements (e.g., datum lines), rhythms or bay systems of adjacent buildings as they are expressed on the facade with adjacent buildings.
- The facade shall have a strong but simple pattern and should maximize window and door openings along the avenue, but need not be rigidly structured for side street elevations.
- Facade compositions should be made up of base, middle and top levels that are readily discernible from each other.
- The base level of a facade should sit on a small plinth that creates a subtle transition between it and the ground.
- The top level of a facade should be noticeable enough to provide a visual cap to the building overall.
- Buildings that are squat in proportion or have strong horizontal element in its facade, such as long rows of windows across facade, are discouraged.
- Vertical elements on the facade, such as columns, posts and pilasters, can be used to heighten an otherwise short building. However, these items should be proportioned so as not to appear too thin or spindly to carry the weight above them.
- The rhythm of the facade should be of a clear, simple and easily recognizable pattern. Within this pattern, however, more subtle and interesting patterns may be utilized to further enrich the design.
- Breaks and fluctuations in the overall rhythm of a building should be used to draw attention to important areas such as the entry, or to simply relieve the monotony of a very long facade.
- Front porches may be used to create a strong rhythm and provide shelter for pedestrians.

SITE ISSUES***Transportation and Mobility***

- Consolidation of entrances and exits to area establishments and a reduction in the number of turns and their associated curb cuts;
- Creation of internal roads, if feasible, to provide secondary access to area establishments for customers, residents, service and emergency vehicles.

Linkage and Curb Cuts

- Establishments are encouraged to share parking areas, common entrances and exits, and to provide sidewalk connections between them.

Parking and Circulation

- Smaller shared parking areas are preferred and should be encouraged to the maximum extent possible.

Open Space and Amenities

- Provide public and semi-public spaces such as plazas, outdoor dining courts, fountains, sculpture and other amenities at key locations on the site to create an attractive, “human-scale” sense of place.
- Open space, courtyards, and landscaped pathways which relate to human scale shall be encouraged to provide transitional areas between public and private spaces. Site improvements shall include walkways, plantings, lighting and other elements deemed necessary by the appropriate review board.

Lighting

- Generally, lighting fixtures for parking lots shall be between 15-25 feet in height, depending on the setting and should not exceed 30-feet in height.
- Metal halide lights may be used for parking lot lights.
- High pressure sodium lights shall be used in pedestrian areas, metal halide lights shall be avoided in these areas.
- Pedestrian lighting shall be on fixtures not exceeding 15-feet in height. Generally, light

sources shall be relatively low to the ground in order to remain in scale with human dimensions and provide illumination beneath the canopy from trees. These can be freestanding fixtures located along the sidewalks. Luminaires without cutoffs are acceptable for pedestrian-level lights however, as a general rule all outdoor lighting shall be screened by shields or hoods to prevent glares onto adjacent premises.

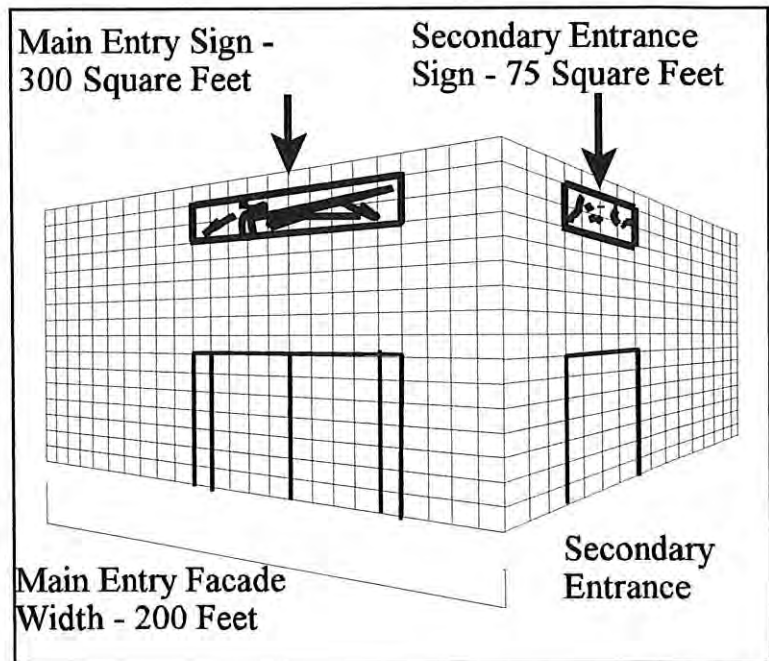
Signage

The following performance standards apply.

Signage Performance Standard for Larger Buildings. For buildings primarily in retail use where the building width along the main entry facade exceeds one hundred (100) linear-feet, such building may be provided with a sign of up to one and one-half (1½) square-feet in area for each linear foot of main entry facade width.

Secondary Sign. For buildings meeting the above-referenced standard, with additional entrances from a building wall other than the main entry facade, or with more than one wall facing the public way accessing the building, one additional sign per entry (up to two) could be permitted. The total square footage of such signs should not exceed twenty-five (25) percent of the permitted main entry sign

In no case should the total signage exceed 600 square-feet.



RESIDENTIAL TRANSITION (MATZ) SUB-AREA

The Monroe Avenue Transitional Zone (MATZ) district generally extends northwesterly along Monroe Avenue from the bridge over the canal at the village/town boundary through French Road and the beginning, immediately past that intersection, of the commercial zone. (See town zoning map for details)

The MATZ sub-area contains a blend of historic and attractive homes and sylvan landscape with attractively landscaped light industrial and high density apartments and condominiums. Sidewalks, street trees and pedestrian-level lighting establishes the edge of the street. The design challenge here is to maintain the green areas and residential character of this sub-area. The desired state of the MATZ sub-area is best defined within the 1999 Monroe Avenue Transition Zone (MATZ) Study Group Visioning Report and the MATZ Overlay Zoning District found in the town zoning code. The desired state of the area is as follows:

- Maintain and enhance the existing residential character and architecture.
- Maintain and enhance property values.
- Extend the look and feel of the historic village to French Road.
- Maintain a preference for residential use.
- Maintain the predominately wooded character and floodplain that helps define this area.

Note: The town zoning code has additional design and use information not repeated herein. Please refer to the code for more specific requirements.

Special Permit Uses

Residential is the preferred use in the MATZ district. Certain uses may be allowed through a special permit process. It will be necessary for any non-residential proposed project to review and demonstrate compliance with these design guidelines and the Town of Pittsford's Zoning Code during the special use permit application process.

ARCHITECTURAL ISSUES

All use conversions/site modifications shall respect the existing architectural and landscape design of the existing residential setting, considering the overall context of the site and building in terms of its history, surroundings, and the aesthetic sense of its original designer and builder. Any improvements shall be completed in a residential style and shall be compatible with the

adjacent structures and neighborhood.

- All use conversions/site modifications, including landscaping, shall feature elements of a residential scale, proportion and landscaping.
- New construction of a residence shall be designed to complement the architectural and landscape design of the district.

Historic Buildings

1. Where the structure being altered is a building of historic significance or listed on a local, and/or state and national register of historic places, adaptive reuse and appropriate additions, as outlined in the U.S. Secretary of the Interior's standards for historic preservation, are strongly encouraged as an alternative to demolition.
2. Where said structure is listed on a local, and/or state and national register of historic places then additional regulations will apply. It is the responsibility of the application to demonstrate compliance with these regulations at site plan review.
3. A certificate of appropriateness from the Historic Preservation Commission is also required prior to any change in the existing structure as referenced in the Town of Pittsford Zoning Law, Article XXX, Landmarks and Historic Districts.

Roof Lines

1. A variety of roof types, heights and gable orientations in proportion with the volume of the building, will help to retain the sub-area's diverse, yet traditional character. However, since virtually all existing principal structures in the study area feature pitched roofs, additions and new construction should avoid flat or very low pitch roofs.

Building Proportion and Scale

1. The size of new construction, the scale of the facade and its materials, including windows and bays, should be small in scale and sympathetic to the scale of its neighboring buildings. Each single-family dwelling shall provide a minimum square foot area on each floor of the dwelling as follows (For descriptions of each dwelling type and method of measurement see section 185-129):
 - Each Type I dwelling shall provide 1,200 square feet on the first floor.
 - Each Type II dwelling shall provide 1,000 square feet on the first floor and 500 square feet on the second floor.

- Each Type III dwelling shall provide 800 square feet on the first floor and 640 square feet on the second floor.

Building Height

1. Within the sub-area 1 ½ to 2 ½ -story residential buildings are preferred. The maximum permitted height of any structure or dwelling is 30 feet, except that chimneys attached to such structures may extend five feet above the highest point of the building.

Lighting

1. All exterior lighting fixtures shall utilize incandescent bulbs. Lighting of walks, parking areas shall be accomplished with low-level lighting in a residential style. Maximum height of any free standing light fixture shall be eight feet. No light source shall be so positioned or installed so as to cause glare or spillage into neighboring property beyond that normally associated by residential use.

Signage

1. For any permissible home occupation or special permit use, one small, free-standing sign, made of wood or non-glare finish material may be erected. Such sign may not exceed three-square feet in area, nor exceed four feet in length in one dimension. Lighting of such sign is permitted by direct illumination (no backlit, or translucent signs permitted), provided the lighting source is entirely screened from view. The sign post may not exceed five feet in height. (The provisions of section 185-134. Signs in residential districts also apply to this district).

SITE ISSUES***Transportation and Mobility***

1. Parking. All parking facilities shall:
 - Be located behind the front façade line of the principal residential structure on the lot.
 - Be suitably screened with plantings and/or fencing.
 - Have a maximum number of parking spaces of 8 (eight) parking spaces per parcel.
2. Traffic generation. No use shall exceed an average daily traffic generation of 20 vehicle trips per day. Trip generation shall be determined by the planning board using published professional manuals and other reliable sources.

The planning board may require vehicle or pedestrian interconnections between properties subject to a special permit review in order to protect the safety of the public and to reduce congestion on Monroe Avenue.

Change of Use

No specially permitted use may be changed to any other use without review of such change by the code enforcement officer. Such review shall determine that the proposed new use would be allowed as a specially permitted use and to what extent, if any, mitigation of the impact of such use would be necessary. The code enforcement officer may counsel with and receive guidance from planning board to make these determinations.

TOWN OF PITTSFORD-BRIGHTON GATEWAY SUB-AREA

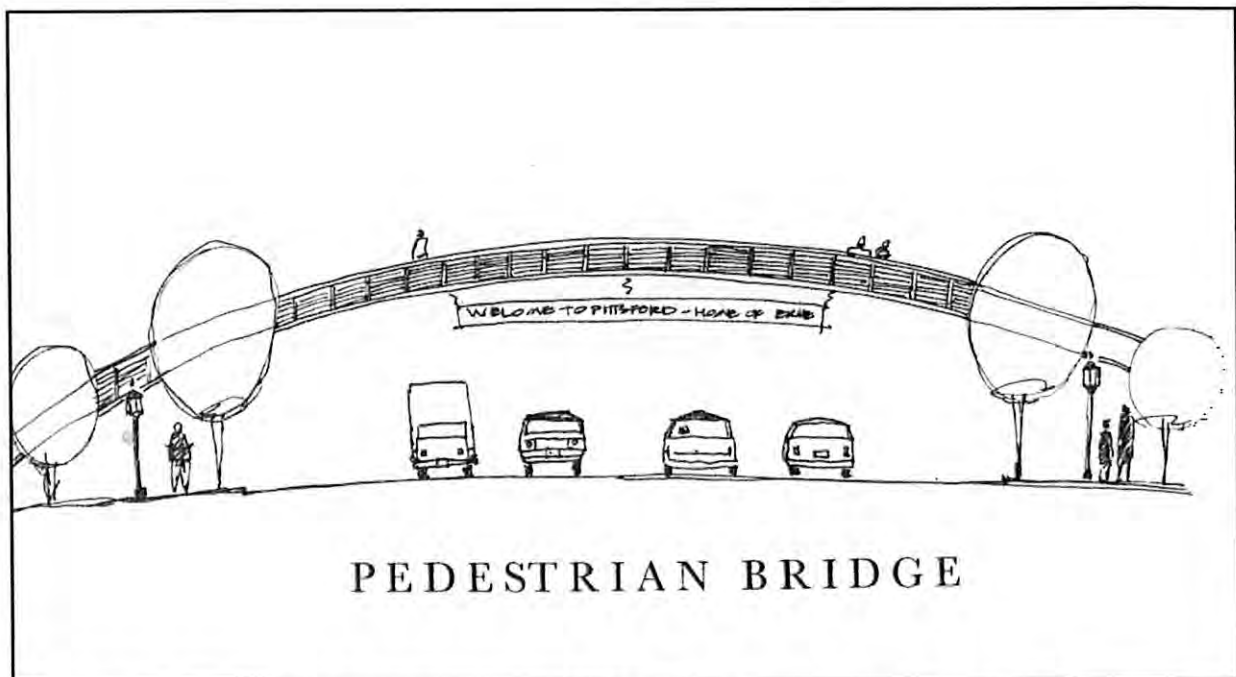
The following improvements are recommended.

Short term:

- Plantings and landscape improvements along street frontage and at intersection of Monroe Avenue and Clover Street.
- Install a “Welcome to the Town of Pittsford” sign at an appropriate location near the Spring House.

Long term:

- Create canal era-themed pedestrian bridge over Monroe Avenue linking the Old Erie Canal Trail with the Auburn Rail Trail.



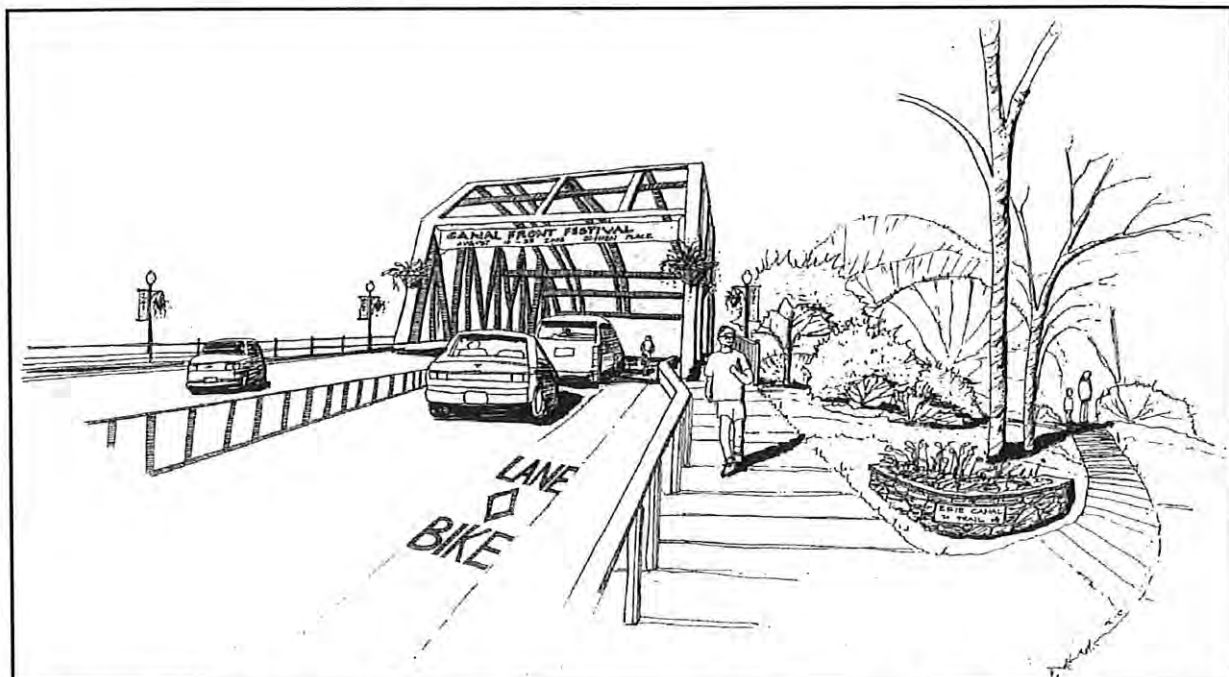
A pedestrian bridge will safely link together the Old Erie Canal and Auburn Rail Trails.

VILLAGE GATEWAY SUB-AREA

The following recommendations will help make this entryway more safe and attractive.

Short Term:

- Relocate the 30 mph speed limit sign from the bridge area to a location at the bottom of the hill near the bridge approach. Remove and consolidate other signs (see sketch).



Improvements to the village gateway area such as plantings, decorative street lights, banners and signage add to the lively atmosphere of this important transition area.

- Remove cobra head streetlights and replace with new directive village-style lighting.
- Mount two sets of planters and/or seasonal holiday decorations, as appropriate, on the bridge facing both directions of traffic for a total of four planters/holiday decoration groupings.
- Mount hanging planter(s) over the sidewalk or on lighting standards.
- Create a new entry statement at either end of the bridge through plantings and a “Welcome to the Village of Pittsford” sign.
- Extend street tree plantings along both sides of Monroe Avenue until the guide rails at the top of the embankment. Tree species used near the gateway areas should be different than those

predominately used along the Avenue, such species should be simple, distinctive and low maintenance, such as flowering crabapples.

- Hang banners from the top of the bridge to announce special events in the Town of Pittsford, such as the Ryder Cup, the Candlelight Night, and the Canal Festival. One banner should be hung facing each direction.
- Narrow Monroe Avenue to two 12-foot lanes, one each direction, and stripe the median at the bridge. If feasible, landscape the medians, in the bridge approach zones.
- Provide bike lanes on both sides of the Avenue and bridge.
- Paint metal work on bridge and railroad crossing a new dark green color.
- Provide pedestrian connections to the Erie Canal Trail on both sides of bridge if possible.

Long Term:

- Bury above-ground utility lines.
- Install curb at the outer-edge of the bike lane and remove the guide rail if feasible.

CONCLUSION

Project designers and board members responsible for project review, should consider the larger context and highest qualities of the area, neighborhood and community when designing and reviewing new construction in the corridor area. Where needed, professional services should be engaged to assist in the creation of attractive, human-scaled places. Communication with professional staff from the town early in project review, including review of these guidelines is strongly recommended.

In addition to the private sector actions outlined within the design guidelines, several public sector actions need to be taken to encourage continued investment, livability, and transformation of the Monroe Avenue corridor. The following are recommended public sector actions to be undertaken by the Town of Pittsford.

- Amend the town code to facilitate the transformation of the corridor to its desired state. Specific areas where the code should be amended include area setbacks, especially in the Old Erie Canal Sub-area. Refinements may also need to be made to the Town's policies for signage regulations, location of and standards for trails, and lighting (such as illumination levels, types of bulbs, fixtures, etc...).
- Coordinate physical improvements to Monroe Avenue (such as sidewalks and pedestrian lighting, in particular in the MATZ area, with the "Main Street for Monroe County" working group (City of Rochester, Town of Brighton, Town and Village of Pittsford) and with the New York State Department of Transportation. Suggested physical improvements include: provision of bus pull-offs and shelters; burial of above-grade utility lines; construction of a canal era-themed pedestrian bridge over the Avenue linking the Old Erie Canal Trail with the Auburn Rail Trail; and, the installation of a tree-lined boulevard along the entire length of the Avenue (where feasible) with planted medians of sufficient width and presence to separate the flow of traffic and provide for the safe, efficient movement of pedestrians.



Graphic depicting a pedestrian friendly streetscape and the transformation of Monroe Avenue into a tree-lined boulevard with planted medians.

- Enter into partnership with the owners of the Spring House, the Oak Hill Golf Course, and the New York State Office of Parks, Recreation, and Historic Preservation to construct a new mineral spring and pavilion on the Spring House site.

- Revisit and refine the plans for the Old Erie Canal and Auburn Rail Trails. As part of this work, develop an interpretative plan for the Old Erie Canal from the canal bed located behind the Spring House to Lock 62. Such a plan shall be phased to include signs, restoration of the former towpath, and future boat or canal demonstrations and connections to Lock 62.